

2nd Avenue

PEDESTRIAN ENHANCEMENT
ALTERNATIVES REPORT

Adams Street to Roosevelt Street

June 2003

ACKNOWLEDGEMENTS

Many thanks to the Core Group members who helped make this document possible:



Core Group

Maureen DeCindis
Maricopa Association of Governments

Phil Jeselnik
Arizona Department of Transportation

Richard Benitez
City of Phoenix

Gail Brinkmann
City of Phoenix

John Siefert
City of Phoenix

Joy Mee
City of Phoenix

Chris Andres
City of Phoenix

Caryn Logan Heaps
A Dye Design

Jackie Keller
Logan Simpson Design

Diane Simpson Colebank
Logan Simpson Design

Ashley Kowallis
Logan Simpson Design

Brad Remme
Logan Simpson Design

Michael Book
Logan Simpson Design

Don Keuth
Phoenix Community Alliance

Brian Kearney
Downtown Phoenix Partnership

Rod Tomita
Downtown Phoenix Partnership

Andrew George
Roosevelt Action Association

Conni McDonough
Resident Representative

Mike Valder
Business Representative

Kris Lowrey
Developer Representative

Rob Lemke
Aztec Engineering

Kevin Robertson
Aztec Engineering

Table of Contents

1.0 Introduction	1
2.0 Pedestrian Assessment Recommendations.....	1
3.0 Project Funding.....	4
4.0 Project Elements.....	16
5.0 Cost Estimate	19
6.0 Schedule.....	23
7.0 Plan Sheets	23
APPENDIX.....	31

Exhibits

Exhibits 1-A and 1-B. Vicinity Map	2-3
Exhibit 2. Section One – Adams to Fillmore (Depicting Parallel On-Street Parking and Interim Trees).....	21
Exhibit 3. Section Two – Adams to Fillmore (Depicting Landscape and Built Shade).....	21
Exhibit 4. Section Three – Fillmore to McKinley (Depicting Double-row Tree Shade and Parallel Parking)	22
Exhibit 5. Section Four – McKinley to Roosevelt (Depicting Double-row Tree Shade and Parallel Parking)	22
Exhibits 6-A thru 6-G. Plan Sheets.....	24-30

Tables

Table 1. Core Group Policy Recommendations	5-9
Table 2. Core Group Guideline Recommendations	10-15
Table 3. Parking Study Table – Adams Street to Fillmore Street.....	17
Table 4. Estimated Project Costs by Element – Adams Street to Roosevelt Street	20

1.0 INTRODUCTION

The physical limits and boundaries of the 2nd Avenue Pedestrian Enhancement corridor are bounded by Adams Street on the south and Roosevelt Street on the north (see Exhibits 1-A and 1-B).

The corridor is composed of two relatively different character areas. The project corridor links the urban, downtown core area of Phoenix to a neighborhood setting, with historically significant properties. To develop a strategy that responds to both of these very different character areas, it is important to accurately define the limits of each area. The urban core area is a very different pedestrian environment than that of the neighborhood area, and the streetscape must reflect and respond to those two unique environments.

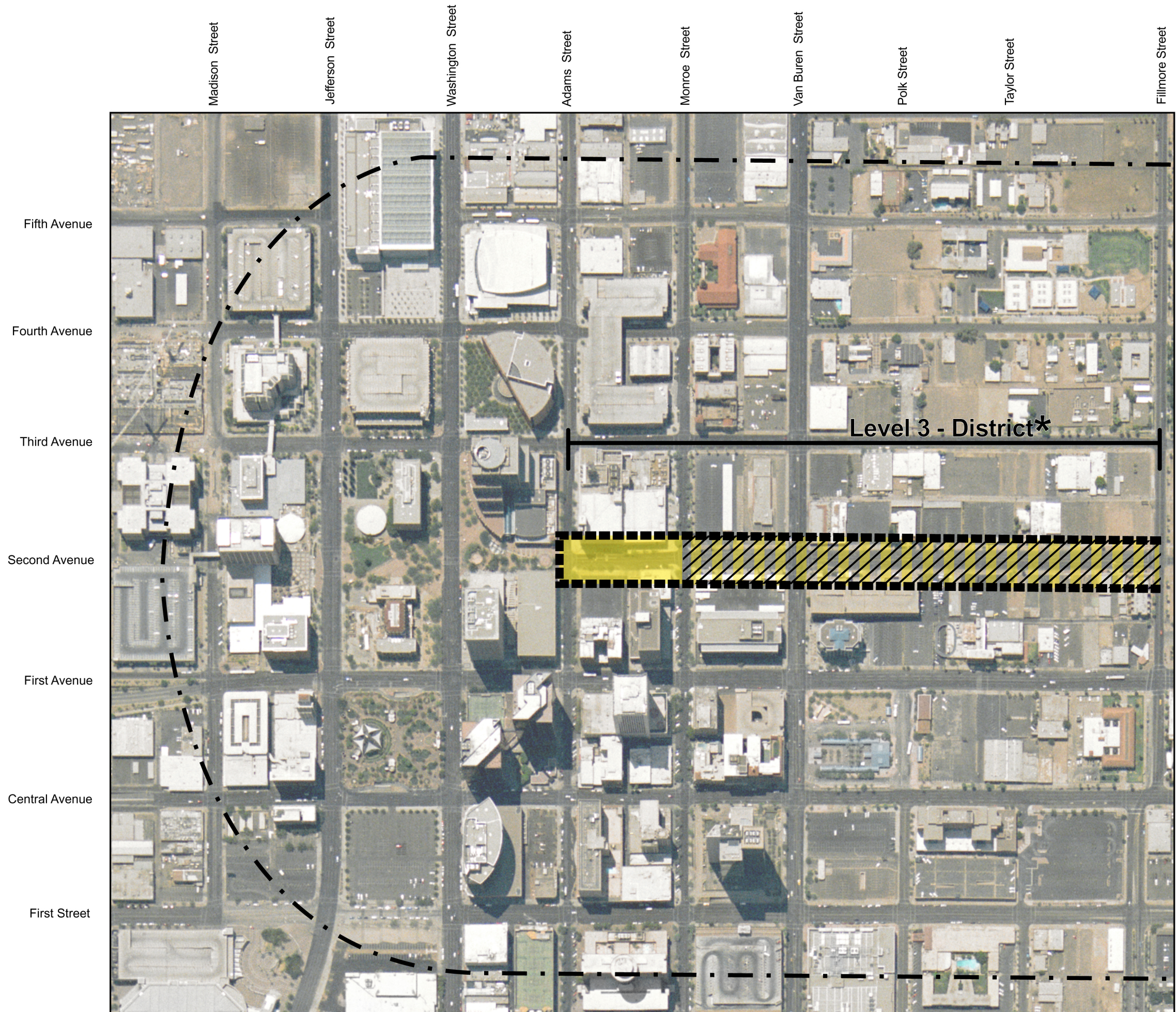
In this study, we refer to the neighborhood area as the “Community” area, while the more urban areas will be referred to as the “District” area. When the Community title is used, it is in reference to the areas along the 2nd Avenue corridor between Roosevelt Street and Fillmore Street. This area has a strong residential character and community feel to it; thus any future improvements must enhance this established character. When the District title is used, it will be in reference to the areas along the 2nd Avenue corridor between Fillmore Street and Adams Street. The District area is more urban in character and will require a different design approach than that of the Community area. These character areas are also identified on Exhibits 1-A and 1-B.

2.0 PEDESTRIAN ASSESSMENT RECOMMENDATIONS

As a response to the Maricopa Association of Governments (MAG)-sponsored *Pedestrian Assessment* for 2nd Avenue, a Core Group was formed. Interested parties, or stakeholders, were identified by the City of Phoenix, and representatives from those stakeholders were asked to participate in the Core Group. The *Pedestrian Assessment* Core Group was a decision-making body made up of the Study Team and these stakeholders. The Core Group authored a series of recommendations regarding possible solutions for improving the pedestrian environment within the corridor area. These recommendations have been inventoried and categorized into 18 areas of focus in accordance with the MAG *Pedestrian Area Policies and Design Guidelines* (MAG *Guidelines*, 1995) and are detailed in this *Alternatives Report*.

The purpose of this document is to identify fundable project elements for the entire project area that will improve the pedestrian corridor based on the Core Group’s recommendations. Some recommendations require modifications to existing private structures or facilities to accommodate intended design recommendations, or involve influences to private development or redevelopment that has not yet occurred, or suggest dissemination of information by public groups. Because of this, the Core Group suggests that the more complex policy recommendations be considered as long-range requirements for a successful pedestrian environment, and the constructible design elements that are within the public right-of-way be considered as a highest priority project.

For the recommendations that are more long-term and complex, responsible parties have been identified and are listed in Tables 1 and 2. These parties will be responsible for overseeing the development and/or implementation of the remaining recommendations.



Key

- MAG Study Funding and HII *Report* Study Limits - \$50,000 (Original MAG Grant - Adams to Fillmore Street)
- Current TEA 21 (ADOT) Construction Funding Limits - \$500,000 (Monroe to Fillmore Street)
- City of Phoenix Construction Funding Limits - \$833,868 (Construction Matching-Monroe to Fillmore Street)
- 2nd Avenue Corridor
- MAG *Pedestrian Assessment* Study Area

*MAG *Pedestrian Area Policies and Design Guidelines* assessment of Adams to Fillmore Street is a MAG Level 3-"District" Area Type. For further definition refer to the MAG *2nd Avenue Corridor Pedestrian Assessment*, February 2003.

Note: All construction funding limits are within the 2nd Avenue right-of-way

Total Construction Dollars - \$1,333,828 (Monroe to Fillmore Street)

Exhibit 1-A. Vicinity Map
 2nd Avenue Pedestrian Enhancement
 Adams Street to Fillmore Street
 Alternatives Report



Fifth Avenue
Fourth Avenue
Third Avenue
Second Avenue
First Avenue
Central Avenue
First Street

Fillmore Street

McKinley Street

Roosevelt Street

Portland Street

Latham Street

Papago Freeway

Culver Street



Key

- MAG Study Funding and HII *Report* Study Limits - \$50,000 (Original MAG Grant - Adams to Fillmore Street)
- 2nd Avenue Corridor
- MAG *Pedestrian Assessment* Study Area

*MAG *Pedestrian Area Policies and Design Guidelines* assessment of Fillmore to Roosevelt Street is a MAG Level 3-"Community" Area Type. For further definition refer to the MAG *2nd Avenue Corridor Pedestrian Assessment*, February 2003.

Note: For current construction funding limits see Exhibit 1-A, page 2.

Exhibit 1-B. Vicinity Map
2nd Avenue Pedestrian Enhancement
Fillmore Street to Roosevelt Street
Alternatives Report



In the following tables, (Table 1 and Table 2), the MAG-sponsored Core Group's recommendations have been inventoried and categorized into the following 18 areas of focus and in accordance with the MAG *Pedestrian Area Policies and Design Guidelines* (MAG Guidelines, 1995).

- | | |
|------------------------------|---------------------|
| • Security | • Traffic Calming |
| • Planning | • Walkway Character |
| • Priority of Implementation | • Walkway Shade |
| • Community Participation | • Parking |
| • Pedestrian Education | • Lighting |
| • Walkway Width | • Signs |
| • Walkway Separation | • Bicycle Access |
| • Walkways | • Transit Access |
| • Intersections | • Pedestrian Routes |

The 2nd Avenue Pedestrian Enhancement recommendations are shown in the following tables (Table 1 and 2). These tables serve multiple functions:

- Listing of all recommendations made by the Core Group as part of the *Pedestrian Assessment* for this study
- Establishing the location affected by each recommendation (Community or District area)
- Recording the associated recommendation number from the *Pedestrian Assessment* document
- Identifying the responsible party or agency to carry the specific recommendation forward

The “Responsible Party” column in the tables identifies acronyms or other abbreviations depicting the following agency/party:

- MAG – Maricopa Association of Governments
- COP – City of Phoenix
- ADOT – Arizona Department of Transportation
- PCA – Phoenix Community Alliance
- DPP – Downtown Phoenix Partnership
- NIA – Neighborhood Initiative Area
- RAA – Roosevelt Action Association
- County – Maricopa County
- SD – School District
- Private – Private developers/entities

The review of the Final Draft version of this document by the City of Phoenix Historic Preservation (HP) Department, yielded a formal response letter that included suggestions for modifications to some of the Core Group's recommendations regarding parking, sidewalks, lighting, and mid-block crossings (see the Appendix). After informal discussions with City of Phoenix HP and Street Transportation Department personnel, the suggested modifications accompany the pertinent items in Section 4.0 PROJECT ELEMENTS listed in this document.

3.0 PROJECT FUNDING

The City has been successful in obtaining a federal TEA-21 grant as a source of partial funding for the design and construction of the 2nd Avenue enhancements between Monroe and Fillmore Streets. The second funding source for the segment between Monroe and Fillmore Streets will be the City of Phoenix itself (see Exhibits 1-A and 1-B). Because of restrictions placed on the use of TEA-21 funds, the City of Phoenix will pay for, exclusively, any element or feature not covered by federal funding. As a general rule, any funding supplied in association with the federal funding source may be applied to structures or facilities directly related to the enhancement or to improvements to pedestrian and landscape areas. Such improvements include lighting, sidewalks, landscaping, street furnishings, and relocation of utilities. The limitations placed on the local, or city, funding is less restrictive. Typically, local funds will be applied to improvements not generally associated or covered by the federal funds. In the case of 2nd Avenue, the local funds will pay for roadway resurfacing, subgrade preparation, demolition activities, and construction of parking facilities.

TABLE 1 – CORE GROUP POLICY RECOMMENDATIONS

Category	Policy Number	District	Community	Core Group Recommendation	Responsible Party
Security	P1	X	X	Continue regular car, bike, horseback, and foot patrols by Phoenix police, Block Watch, private security at the YMCA, and bank security forces. Continue direct communication with police such as the focus groups. Establish a liaison for all security groups within the area for information exchange and patrol overlaps. Encourage manned building security, such as doormen, for high-end mixed-use residential developments. Continue to publicize improving crime rates to potential buyers and developers.	COP DPP RAA Private
	P2	X	X	Continue the current system of graffiti eradication/prevention.	COP DPP
	P3	X	X	As pedestrian numbers increase and properties infill, provide direct police phone lines for added security if crime statistics remain unacceptable. This could be in the form of emergency buttons at doorways such as those used at the Capitol.	COP
Planning	P4	X	X	Continue applying existing guidelines consistently to new development and renovations. Continue existing collaborative review process of proposed developments. Establish guidelines, budget, and schedules for new and renovation work within the right-of-way. Obtain Council approval. Study existing zoning and overlay district regulations for contradictions and gaps. Establish regular lines of communication between resident/owners and enforcement bodies. Reinforce Copper Square materials, development types, and guidelines in the right-of-way. Develop a character guideline that respects the historical precedent for the Community area (north of Fillmore Street) in the right-of-way.	COP PCA MAG DPP RAA
	P5	X	X	Endorse transportation-oriented development (TOD) ideas, when approved, and interface this set of policies and guidelines with the TOD as a joint ordinance. If appropriate, allow for nontraditional big-box, mixed-use retail development (south of Fillmore Street) as stated under the current zoning requirements.	COP
	P6	X	X	Conduct community/stakeholder outreach to establish theme or character. <i>Note: This policy item is part of the purpose of this funded study and project and will be a phase in the establishment of this character development.</i> Obtain Council approval.	COP
	P7	X	X	Maintain a view corridor in the District portion (south of Fillmore Street) for the Orpheum Theatre on its north side. This should not conflict with current ordinances or regulations.	COP

Category	Policy Number	District	Community	Core Group Recommendation	Responsible Party
Planning	P8	X	X	Maintain the traffic signal on Van Buren Street. Establish four-way stops on all east/west street intersections rather than stoplights. Maintain that there be no right- or left-turn lanes along the Corridor or at the cross streets onto the Corridor.	COP
	P9	X	X	Site new building construction adjacent or close to the edge of the sidewalk in the District Area (south of Fillmore Street). Encourage developers to vary setbacks within range, depending on the use. Design boundary of right-of-way and private space to have continuity in the District Area (south of Fillmore Street) by having paving types and patterns blend with each other. Match setbacks to those of existing historic properties in the Community area (north of Fillmore Street); clearly define where public/private space ends (at the right-of-way). Conduct a case-by-case study on setback requirements between Fillmore and McKinley Streets (the transition block).	COP
	P10		X	Establish four-way stops on all east/west street intersections rather than stoplights. Leave signaled crossing time as it is until substantial pedestrian use warrants programming more time at the Van Buren Street signal for pedestrians to clear the corner. Install "countdown" types of signal timing indicators (40-second crossing time limit).	COP
	P11		X	Conduct a public outreach to define a character theme for the Community Area (north of Fillmore Street) that preserves and enhances historic resources. Apply vision for character to the right-of-way development. Research the historic plat map for restoration information. Establish a pedestrian-friendly open space that joins the Downtown Core to Hance Park. <i>Note: These policy items are part of the purpose of this funded study and project and will be a phase in the establishment of this open space and character development.</i>	COP DPP NIA RAA PCA
	P12		X	Enforce existing zoning requirements for the Downtown Core for new and renovated buildings. Encourage adding openings or other visual interest/animation on existing buildings that do not have a high proportion of openings in the building facades of the Phoenix Main/AT&T Communications building, Phoenix Personnel building, and two parking garage structures.	COP DPP PCA

Category	Policy Number	District	Community	Core Group Recommendation	Responsible Party
Planning	P13	X	X	Consider revising the current sidewalk vendor requirement to allow special event vendors in the historic portion of the Community Area (north of McKinley Street); allow special event and temporary vendors in the transition portion of the Community Area (between McKinley and Fillmore Streets); and continue to allow sidewalk vendors as currently allowed in the District Area (south of Fillmore Street). Locate sidewalk vendors specifically within the “node” portions of the sidewalk; do not allow them to interfere with the clear walkway width of any sidewalk. Supply temporary-use electrical plug-ins in the right-of-way for vendors.	COP
	P14	X	X	Endorse the three land use types summarized in the <i>2nd Avenue Corridor Pedestrian Assessment</i> as recommended in the <i>HII Report</i> , i.e., 1) Low-density, Multi-Family Residential (between Roosevelt and McKinley Streets); 2) Medium-Density, Mixed-Use Residential (between McKinley and Polk Streets); and 3) High-Density, Large-Scale Mixed-Use (between Polk and Adams Streets). Encourage private development within the Corridor to be viable for the mixed-use zoning category with pedestrian-scale features that allow for “festival-type” commercial elements along with residential components.	COP
	P15	X	X	Provide policies or stipulations that retail entrances should front onto pedestrian routes. The transportation-oriented development (TOD) overlay may require/recommend this criterion.	COP
	P16	X		Look for and encourage private uses in existing parking lots, such as flower and bake sales. Encourage clustering of buildings to maximize pedestrian connections between uses.	COP RAA Private
	P17	X		Continue to allow existing parking as a temporary use on vacant parcels or provide more enlivening uses on the vacant parcels to provide some activity. Enforce dust control requirements.	COP County
	P18	X		Encourage development of temporary (periodic art show, special event, festival, and/or sidewalk art vendor) and permanent (annual special events, corridor-wide theme) art and monuments in the streetscape in the District and Community Areas. Interpret existing and former historic aspects of the Corridor such as the Orpheum Theatre and the historic properties/neighborhood. Establish a liaison and program with the Phoenix Arts Commission.	COP NIA RAA
	P19	X		Encourage retail and all other development along the Corridor in the District Area (south of Fillmore Street) to have a length of no greater than 35 feet without openings (windows), or create pedestrian access (doors) at this spacing.	COP
	P20	X		Require retail and all nonresidential first floor development along the Corridor in the District Area (south of Fillmore Street) to have transparent windows.	COP

Category	Policy Number	District	Community	Core Group Recommendation	Responsible Party
Planning	P21	X		Work together to include the entire Corridor in existing brochures. Conduct study with adjacent stakeholders on what to celebrate; educate out-of-area visitors about what is important in the Corridor. Provide interpretive sign package design guidelines.	COP MAG PCA DPP NIA RAA Private
	P 22	X		Coordinate with tourist agencies and car rental and hotel information mapmakers, when appropriate development is in place, to include this Corridor as a destination place. No recommendation is given for interstate transportation guidance signs on freeways.	COP ADOT
Priority of Implementation	P23	X	X	Determine the first priority retrofitting of improvements as part of the next phases in the project.	-
	P24	X	X	Encourage, where appropriate, inclusion of the Corridor in Copper Square marketing efforts. This will become particularly important as 2 nd Avenue develops into a connection between Hance Park and the Downtown Core and all the activities that are in both areas. The current marketing programs to attract visitors and tourists are presently sufficient.	DPP NIA PCA
Community Participation	P25	X	X	Facilitate potential merchant wishes to form an association for coordinating sponsorship and participating in the celebration of current events programs and in creating others. This will become particularly important as 2 nd Avenue develops into a connection between Hance Park and the Downtown Core and all the activities that are in both areas.	DPP NIA RAA Private
	P26	X	X	<u>Extended hours of business will probably occur naturally as the Corridor develops, and no further recommendation is necessary.</u>	-
	P27	X	X	Allow places for temporary and permanent art in the right-of-way as described in P13 – Planning.	COP
	P28		X	Establish a merchant's association, once there are more merchants, for the Corridor, possibly as an adjunct to the existing Copper Square Retail Council.	COP DPP PCA
	P29		X	Program small temporary events around existing larger events such as the RAA Historic Home Tour.	PCA DPP RAA
	P30		X	Incorporate a descriptive brochure of the Corridor on the Web. Coordinate Web links with other event and real estate Web sites.	PCA DPP RAA
	P31	X		Provide good east/west connections from the Corridor to museums, restaurants, and nightclubs. Enhance connections from the Corridor to existing and future restaurants, nightclubs, and cultural attractions on both Adams Street and Monroe Street.	COP

Category	Policy Number	District	Community	Core Group Recommendation	Responsible Party
Pedestrian Education	P32	X	X	Undertake a descriptive brochure, a video, oral histories project, a Web site, a walking map, informal talks, and/or other means to educate prospective developers and new residents in the Corridor Community Area (north of Fillmore Street). Provide or add to Copper Square information a descriptive brochure, a video, oral histories project, a Web site, and/or a walking map for the Corridor District Area (south of Fillmore Street) for potential developers and residents.	PCA DPP NIA RAA Private
	P33	X	X	Implement or offer a system of consistent interpretive signs to owners of historic properties in the Roosevelt Historic District within the Corridor. Celebrate historically significant properties and properties that no longer exist (by means of a plaque or other mean) in both the Community (north of Fillmore Street) and District (south of Fillmore Street) areas.	COP NIA RAA Private
	P34		X	Encourage the City of Phoenix to sponsor a Safety Program, Safe Route to School, Walking School Bus, Red Sneaker Day, or other such annual programs in conjunction with the School District and Parent-Teacher Association.	RAA NIA SD Private

END OF TABLE 1 – CORE GROUP POLICY RECOMMENDATIONS

TABLE 2 – CORE GROUP GUIDELINE RECOMMENDATIONS

Category	Guideline Number	District	Community	Core Group Recommendation	Responsible Party
Walkway Width	G1		X	Provide a 10-foot- wide effective walkway width (8-foot minimum) in the Community Area (north of Fillmore Street) in accordance with the cross section (Exhibits 4 and 5).	COP
	G2	X	X	Provide a 15-foot effective walkway width (13-foot minimum) in the District Area (south of Fillmore Street) in accordance with the cross sections (Exhibits 2 and 3). Seek to mitigate increased heat and runoff caused by wider sidewalk areas.	COP
Walkway Separation from Traffic	G3	X	X	Provide metered parallel parking (1 to 2 hours), staggered and interrupted, as described in the <i>2nd Avenue Corridor Pedestrian Assessment</i> . Resolve, in the design phase, the issue of high curbs as an impediment to opening doors. Provide a landscaped buffer that matches the existing adjacent buffer in the Community Area (north of Fillmore Street) in accordance with the cross sections (Exhibits 4 and 5). Provide a buffer from the effective walkway area to the curb in the District Area (south of Fillmore Street) in accordance with the cross sections (Exhibits 2 and 3).	COP
Walkways	G4	X	X	Provide no new traffic signals, but keep the one at Van Buren Street. Keep button timing as is, but ensure it is up to current Americans with Disabilities Act <i>Accessibility Guidelines</i> (ADAAG) requirements for audibility and push force needed. Install “countdown” types of signal timing indicators (40-second crossing time limit) to the existing Van Buren Street/2 nd Avenue traffic signal.	COP
	G5	X	X	Investigate whether crossing time is adequate for the expected type of pedestrian. Provide longer crossing times at the Van Buren Street signal.	COP
	G6	X	X	Investigate potential for corner radii treatments on Fillmore Street and Roosevelt Street. No traffic calming is necessary along 2 nd Avenue, and traffic calming is not possible along Van Buren Street.	COP
	G7	X		Add stop signs along Adams Street at 2 nd Avenue.	COP
	G8	X	X	Discuss programmed, integrated art, and historic monumentation for the Corridor with the City of Phoenix Arts Commission.	COP
	G9	X	X	Resolve the drainage problem at Fillmore Street and 2 nd Avenue to remove pedestrian impediments.	COP

Category	Guideline Number	District	Community	Core Group Recommendation	Responsible Party
Walkways	G10	X	X	Encourage alleyway access for automobiles and delivery vehicles where possible. Develop and pave parallel alleyways on the east and west sides of 2 nd Avenue. Eliminate curb cuts and driveways on 2 nd Avenue where possible.	COP
	G11	X		Modify the existing four travel lanes down to two, 11-foot-wide lanes (with 10 feet on either side of the travel lanes reserved for parking) between Monroe Street and Fillmore Street in accordance with cross sections (Exhibits 2 and 3) and the aerial plan (Exhibits 6-A thru 6-D).	COP
	G12	X		Establish 75 percent shade through a combination of built shade structures and shade trees (measured at maturity, between palms as necessary) along all routes in the District Area (south of Fillmore Street), including east-west streets and at nodes such as building entries, pocket parks, and intersection corners as stated in G27 – Walkway Shade.	COP Private
	G13	X		Consider “cool connector” concepts at intersections. <i>Note: “Cool connectors,” specifically shading canopies and pedestrian amenities, are part of a research effort in passive-cooling strategies for desert cities led by the School of Architecture at Arizona State University.</i>	COP
Intersections	G14	X	X	Ensure that all crosswalks within the Corridor conform to the latest Light Rail Transit, ADA, and Americans with Disabilities Act <i>Accessibility Guidelines</i> (ADAAG) requirements.	COP
	G15	X	X	Consider bulb-type curb extensions as part of the parallel parking bays at Roosevelt, McKinley, Fillmore, Monroe, and Adams Streets. See the aerial plan (Exhibits 6-A thru 6-G).	COP
	G16	X	X	Provide two mid-block crossings (such as ramps or striping) between Fillmore Street and Van Buren Street that define the interrupted grid (Polk and Taylor Streets). See the aerial plan (Exhibits 6-C and 6-D).	COP
Traffic Calming	G17	X	X	Consider a raised crosswalk on 2 nd Avenue at Adams Street and Fillmore Street to slow traffic at these intersections. <i>(At the proposed mid-block crossings, raised crossings were considered by the Core Group and were rejected).</i>	COP
	G18	X	X	Consider bulb-type curb extensions as part of the parallel parking bays at Roosevelt Street, McKinley Street, Fillmore Street, Monroe Street and Adams Street. See the aerial plan (Exhibits 6-A thru 6-G). Consider providing vertical visual treatments to alert motorists by announcing entry into a pedestrian zone along the cross streets.	COP
	G19	X	X	<u>Barriers (chicanes) in the street are not necessary to alert drivers; speeds are already slow.</u>	COP

Category	Guideline Number	District	Community	Core Group Recommendation	Responsible Party
Walkway Character	G20	X	X	Support existing guidelines and requirements to orient building entries toward 2 nd Avenue, at a minimum. If alley access is used, there may be a secondary orientation in that direction as well. If building is a block wide, provide primary access on 2 nd Avenue, not 3 rd Avenue. Seek to work with existing private owners such as the YMCA and Wells Fargo Bank, and new developers, to give visual interest/animate street-level orientation on 2 nd Avenue. Conduct a block-by-block study to determine setbacks.	COP Private
	G21	X	X	Support existing guidelines and requirements to establish a strong sense of entry into each building. Work with future developers to improve their building entries as Corridor nodes, extending into the right-of-way. Respect/restore the historic landscape type in the Community Area (north of Fillmore Street), which consists of palms in a linear, formal pattern, turf in the buffer area, and front yard setbacks.	COP Private
	G22	X	X	Investigate abandonment of the alleys to provide a private drive for the adjacent users and emergency/utility services.	COP
Walkway Furnishings	G23	X	X	Provide trash receptacles typical of Copper Square at 500-foot intervals and at nodes in the District Area (south of Fillmore Street). Provide trash receptacles at nodes, in the Community Area (north of Fillmore Street). The choice of trash receptacles in the Community Area should respect the area's historic character.	COP
	G24	X	X	Provide integral seating opportunities such as walls or ledges within the right-of-way in the District Area (south of Fillmore Street). Provide benches with backs in the right-of-way, if applicable, typical of Copper Square to meet the 500-foot criterion in the District Area (south of Fillmore Street). (Consider 250-foot spacing distance as preferable). Encourage informal seating for dining adjacent to the right-of-way in the District (south of Fillmore Street) or Community (north of Fillmore Street) areas. Provide seating, either bench-type with backs and/or wall-type, at nodes in the Community Area (north of Fillmore Street). The choice of trash receptacles in the Community Area (north of Fillmore Street) should respect the historic character.	COP Private
	G25	X	X	Consider chilled drinking fountains at nodes in the District (south of Fillmore Street) and one in the Community (near McKinley Street).	COP
	G26	X	X	Develop nodes in both Community (north of Fillmore Street) and District (south of Fillmore Street) areas that are adjacent within the right-of-way Corridor. Work with future developers to integrate right-of-way spaces and their own small green spaces and plazas.	COP Private

Category	Guideline Number	District	Community	Core Group Recommendation	Responsible Party
Walkway Shade	G27	X	X	Provide 75 percent shade in the right-of-way of the Community Area (north of Fillmore Street), with mainly vegetation (double row of trees for each sidewalk), as measured at tree maturity. See the aerial plan (Exhibits 6-E thru 6-G). Supplement the minimal shade provided by palms. Provide 75 percent shade in the right-of-way primarily with building shadow and arcades, awnings, or other shade-providing structures in and adjacent to the right-of-way in the District Area (south of Fillmore Street. Additionally, until buildings and built shade are developed along the entire length of the District Area, provide a double row of trees within interim tree wells in each sidewalk with the option to future developers to remove the interim tree wells once built shade is completed. See the aerial plan (Exhibits 6-A thru 6-D).	COP Private
	G28	X	X	Provide staggered, on-street parallel parking as described in the <i>2nd Avenue Corridor Pedestrian Assessment</i> . See the aerial plan (Exhibits 6-A thru 6-G). Provide adequate evening and overnight parking near the existing multi-family residences. Consider use of consolidated daytime meter boxes.	COP
Parking	G29	X	X	Eliminate the existing diagonal parking.	COP
	G30		X	Locate large parking lots and/or garages at the rear (alley side) of buildings and provide access through the alley.	COP Private
	G31	X		Provide staggered on-street parallel parking. See the aerial plan (Exhibits 6-A thru 6-D).	COP
	G32	X		Encourage developers to provide a dedicated path from their behind-building parking areas or garages to 2 nd Avenue. Either between-building drives/walkways or passage through a lobby is acceptable.	Private
Lighting	G33	X	X	Use typical Copper Square pedestrian and street lighting fixtures within Copper Square boundaries, preferably copper antique fixtures. Consider enhancing traffic light poles at Van Buren Street. Consider adding to lighting near the Orpheum Theatre. In the Roosevelt Historic Neighborhood, use a street light fixture that respects the historic character of the neighborhood. Remove existing nonconforming streetlights as new, replacement streetlights are funded.	COP
	G34	X		Encourage developers to provide specialty lighting on all sides of buildings and storefronts. Provide specialty lighting in landscape bays/nodes. Investigate the use of light displays at celebrations and at special events. Provide connections on light poles for seasonal lighting or decorations.	COP Private

Category	Guideline Number	District	Community	Core Group Recommendation	Responsible Party
Signs	G35	X	X	Provide directional and wayfinding signs that are cohesive in character throughout the Corridor, in both the District (south of Fillmore Street) and Community (north of Fillmore Street) areas. Consider enhancing regulatory signs in the District Area. Provide Copper Square thematic signs within its boundaries. Encourage pedestrian-scale signs in the retail areas of the Community (north of Fillmore Street) and District (south of Fillmore Street) areas. Provide interpretive signs that respect the historic character in the Community Area (north of Fillmore Street). Review existing sign ordinance in the Copper Square/District Area (south of Fillmore Street).	COP DPP Private
	G36	X	X	Use Braille on pedestrian-level signs. Use audible signals and warning paving patterns at intersections. Consider an international sign marking system in the District Area (south of Fillmore Street). Investigate conflicts with Copper Square sign codes.	COP
Bicycle Access	G37	X	X	Provide no designated or striped bike lanes. Bicycles will share the street.	-
	G38		X	<u>No additional bike lanes are needed to provide a safe route to schools.</u>	-
	G39		X	Continue Adams Street bike route connection from 3 rd Avenue to 2 nd Avenue.	-
	G40	X		Encourage developers to provide dedicated bike parking at multi-family residence parking garages (locker type). Provide bicycle parking (loop type) in landscape bays/nodes.	COP Private
Transit Access	G41	X	X	<u>No further transit stops are needed on 2nd Avenue.</u> Investigate adequacy of Van Buren Street, Fillmore Street, Roosevelt Street, and all east/west streets from 1 st Avenue to 3 rd Avenue as connectors to transit.	COP
	G42	X		Consider coordinating temporary tourist bus use of existing vacant parking areas with property owner permission. Plan for coordinating tourist bus parking/shuttle system with current Copper Square shuttling system at the buildout phase.	COP DPP Private

Category	Guideline Number	District	Community	Core Group Recommendation	Responsible Party
Pedestrian Routes	G43		X	Encourage developers to provide a dedicated path from their behind-building parking areas or garages to 2 nd Avenue. Either between-building drives/walkways or through a lobby are acceptable.	Private
	G44		X	Upgrade intersections to comply with ADAAG. Complete routes in this project to provide full and best practices in the District Area (south of Fillmore Street) and minimum standards in the Community Area (north of Fillmore Street).	COP
	G45		X	Provide no transit stops on 2 nd Avenue, but do provide a connection at Roosevelt Street to the future Light Rail Transit station on Central/1 st Avenues at Roosevelt Street.	COP
	G46		X	Provide routes throughout the Corridor with adequate width for shared use by fitness seekers and strollers to adjacent area parks.	COP
	G47	X		Provide an east/west connection to the 3 rd Avenue Phoenix Sonoran Bikeway on Monroe and McKinley Streets.	COP
	G48	X		Provide information signing in regard to the an east/west connection on Adams and Van Buren Streets from 2 nd Avenue to hotels/tourist facilities.	COP DPP

END OF TABLE 2 – CORE GROUP GUIDELINE RECOMMENDATIONS

4.0 PROJECT ELEMENTS

The following elements have been identified as being highest priority due to the requirements and the restriction placed on the funds associated with this project. The inclusion of the following elements reflects the status each element currently holds under the existing funding requirements, that is, they are allowable and approved improvements according to the funding requirements. In addition, these elements are the first steps needed to ensure an improved pedestrian environment for the 2nd Avenue corridor, thus influencing and potentially encouraging future development along the corridor. These project elements, as depicted in the cross sections (Exhibits 2-5), should be designed and built to allow/enable all other recommendation elements to occur during later phases of development with minimal disturbance and added cost.

The Core Group recommends the following project elements as funding allows. Refer to Exhibits 6A-6G.

Curb and gutter – New curb and gutter will be installed on both sides of 2nd Avenue, south of Fillmore Street. North of Fillmore Street, the existing curb and gutter is to remain in place. The new curb will provide a narrowed section of the street with a continuous gutter. The new curb will also define parallel parking areas alternating with landscaped areas, called “landscape bays.” A valley gutter that connects to the adjacent curbside gutter will serve parking bays. Consideration will be made to continue the water-retaining function of the existing curb height (varies from 4 to 14 inches, north of Fillmore Street and a continuous 6 inches south of Fillmore Street), while proving no impediment to the curbside parked auto, the pedestrian, or the mobility-challenged individual.

Parking bays – South of Fillmore Street, parallel parking bays, 8 feet wide, will be provided on both sides of the street, although not continuously. No more than the length of five parking spaces will be provided without a landscape bay division, and no fewer than two parking spaces will be provided for in the parking bays. Parking spaces are recommended to be metered for a maximum of 2 hours between Adams and Fillmore Streets.

A brief study on parallel versus angle parking layouts was undertaken in response to public comment at the neighborhood open house. City of Phoenix standards for types of parking, offsets, and widths were used, and the following table compares the totals identified for each layout in the area between Adams Street and Fillmore Street (District Area) (see Table 3). The existing seating node area lengths, as depicted on the plan at the neighborhood open house, were not to be altered if at all possible; however, it was identified that to maintain the required widths and offset for the angled parking stalls, there would not be sufficient space for the second row of canopy trees to shade the sidewalk.

The Core Group discussed that the open house request for angled parking involved the area north from Adams Street to McKinley Street. However, because of the Roosevelt Historical District designation, the members recommended that north of Fillmore Street, the curb and gutters should stay the same, providing parallel on-street parking. Members of the Study Team clarified to the Core Group members that the curbside parking is being provided for convenience, and not as the result of a needs study. Future development will be required to provide sufficient on-site parking in accordance with City zoning codes and ordinances.

There was an at-length discussion about providing continuity to 2nd Avenue, with all parallel parking versus the opportunity for exceptions on a case-by-case basis. It was suggested that angled parking could be provided at the intersections south of Fillmore Street (such as in front of the Lofts at Fillmore) and parallel parking could be left along the blocks. Among the responses to this suggestion was the point that because pedestrian activity is anticipated at the intersections and because this is a pedestrian study, provisions should be made for the pedestrian first, and then for the motorist. On-street parking is, therefore, being provided for convenience, not as a result of a needs study. In addition to being safer statistically, parallel parking creates less of a visual intrusion than does angled parking because pedestrian viewers see fewer lines associated with varying vehicle lengths and heights. An additional response stated that this is first and foremost a pedestrian street. There will be light rail stations closeby, good bus service, and people living nearby or walking from Downtown who do not necessarily need cars. On-street parking as planned, with additional trees, will reduce the asphalt-generated reflected heat, and be more inviting to pedestrian use.

TABLE 3. PARKING STUDY TABLE - Adams Street to Fillmore Street (District Area)

	Parking Area Type (Parallel and Angled)			
	Parallel	30 Degree Angle	45 Degree Angle	50 Degree Angle
Parking Area Length (ft)	Vehicles per Parking Area			
50	2	2	3	3
75	3	3	5	5
100	4	4	6	7
125	5	6	8	9
Parking Area Width Required (ft)	10	19.25	21.5	22.5
Total parking spots (Adams Street to Fillmore Street)	80	91	126	142
Increase in Parking Spots	-	11	46	62
Percentage increase	-	14%	58%	78%
Increase in Parking Width (ft)	-	9.25	11.5	12.5
Percentage increase	-	93%	115%	125%
Benefit Ratio	-	0.15	0.50	0.62
(Ratio of the additional parking spots gained for the additional width required)				

Note: Minimum street width with angled parking is 23 feet. This would require two, 11.5-foot travel lanes.

The Core Group agreed to maintain its recommendation for parallel parking along the entire length of 2nd Avenue (Adams to Roosevelt), with no exceptions.

Landscape bays – Landscape bays are designed as dividers between parallel parking bays. Typically, landscape bays will be a maximum length of 40 feet and a minimum of 15 feet in length, depending on the location of driveways and available space. The intended purpose for providing landscape bays is to break up the visual impact created by uninterrupted rows of parallel-parked vehicles or of long stretches of vacant parking stalls. Landscape bays will be located along the 2nd Avenue corridor between Adams Street and Fillmore Street. (See *Landscape buffers* for other landscape areas).

Landscape buffers – The areas between the sidewalk and the back-of-curb edge will be landscaped and will function as a buffer between pedestrian and vehicular traffic on 2nd Avenue. In addition to trees, shrubs, and existing turf, these buffer areas may include ADA-accessible hardscape areas, seating areas, water features, overhead structures, and public art or event space. Plant material may also be installed in the hardscape areas by means of planters, either above- or at-grade, and trees planted in grates. Along the entire Corridor length, a double row of shade trees along both sidewalks in the right-of-way is recommended to provide adequate and interim shade, in the District Area, until built shade is constructed. Access to both a power and water source is recommended for areas designed as event space. In addition, such event space will include enhanced paving for future temporary and semi-permanent structures. Such event space is anticipated only south of Fillmore Street, but landscape buffers will exist throughout the project.

Maintenance – The maintenance of the roadway, lighting, curb and gutter, and ramps will be the responsibility of the City of Phoenix Street Transportation Department. The City of Phoenix Parks, Library and Recreation Department will maintain the landscaping. In areas north of Fillmore Street, where existing turf would be allowed to remain between back of curb and the sidewalk, the maintenance will be the responsibility of the city. However, the city will not plant winter perennial rye grass and will not mow or irrigate turf areas during the winter months and the city will mow during the summer on a two-week rotation. Should the adjacent property owner desire to mow, or otherwise improve the turf areas, they would be welcome to do so with notification to the responsible City department.

Sidewalks – Continuous sidewalks will be provided on both the east and west sides of 2nd Avenue. Subsequent to the Core Group's final recommendation for the sidewalk width between Adams and Fillmore Streets to be a 13-15-foot clear width, and the sidewalk width between Fillmore and Roosevelt Street to be a 10-foot clear width, discussions with COP Historic Preservation personnel determined modifications to the Core Group's recommendation. In support of the historical integrity of the Roosevelt Historic District, the sidewalk south of McKinley Street, the effective clear walking area width on the east and west sides of 2nd Avenue will be a consistent 10 feet (see Exhibits 6-A – 6-G). Between McKinley and Roosevelt Streets, on the east side of 2nd Avenue, the effective width of the sidewalk will be 8 feet, which will retain the existing five-foot sidewalk with an additional 3 feet of new pavers and a 5-foot landscape easement to provide for a row of canopy trees. Along the west side of 2nd Avenue, between McKinley and Roosevelt Streets, the existing 5-foot sidewalk width will remain and a 5-foot landscape easement will also provide for a row of canopy trees. These widths will vary at intersections or at areas impacted by utilities or structures. The new sidewalks and ramps will meet or exceed all ADA, COP and MAG standards related to pedestrian travel and access.

Several segments of early twentieth-century sidewalks are present within the project area. These sidewalks have original contractor stamps dating to 1909 and may be potentially eligible for listing in the State and National Registers of Historic Places (see photos located in the Appendix). These historic stamps will be preserved as interpretive pieces and/or incorporated into the new design concept.

Removals – Removals will consist of pavement; curb and gutter; existing light fixtures; regulatory signs; damaged/unstable palm trees (some may be salvaged, see *Planting*); other miscellaneous vegetation; drainage structures and facilities; ramps; and sidewalks. In addition, some utilities may require relocation or layout adjustments to accommodate the intended improvements.

Lighting – Because this is a pedestrian project, adequate lighting is both necessary and required along the entire length of the project corridor. Specified light levels must be achieved on both sides of 2nd Avenue and follow COP pedestrian standards for safety as applied to footcandles for lighting. Subsequent to the Core Group's recommendation, discussions with COP Historic Preservation personnel identified that historically representative decorative, pedestrian streetlights will be necessary within the Roosevelt Historic District (north of Fillmore Street). Streetlight fixtures south of Fillmore Street will blend with the existing Copper Square fixtures.

ADA Ramps – All sidewalks that intersect a roadway with curb and gutter, will use ADA/ADAAG (ADA *Accessibility Guidelines*) ramps. These ramps will meet the current COP and ADA ramp requirements. Two ramps will be installed at each corner. Crosswalk striping will also be integrated and will delineate the pedestrian travel area. Crosswalk striping or special paving will be installed according to COP standards.

Planting – All plant material will adhere to the Arizona Department of Water Resources (ADWR)-approved plant list for right-of-way installations. The landscape theme will include existing palm trees spaced at regular intervals along both sides of 2nd Avenue, throughout the project corridor. Salvageable palm trees from within the corridor and adjacent neighborhood in risk of future removal may be transplanted to fill any gaps in the regular interval pattern of palm trees along 2nd Avenue. Along both edges of each sidewalk, a double row of shade trees will also be installed between the palm trees to enhance available shade during the summer months. In addition to planting trees adjacent to the sidewalks, additional trees will be installed in landscaped areas as either accent trees or to help define the character of the area. Trees that are evergreen are preferred, as are low-litter and thornless varieties. Shrubs and other accent plantings located adjacent to pedestrian areas will be thornless and provide a variety of color, scent, and distinctive forms. Plants that present a potential hazard to pedestrians, because of thorns, shall be installed away from sidewalks, seating areas, and other pedestrian areas. All planting areas will receive an inert groundcover of decomposed granite. The size and grade of decomposed granite will be consistent with the City of Phoenix Street Transportation Department standards. Plantings north of Fillmore Street may include existing turf. Current Arizona Department of Water Resources (ADWR) law indicates that additional turf is not allowed within the right-of-way; however, the Core Group's recommendation is to recommend turf as a groundcover between the back of curb and the inside edge of the sidewalk, thus contributing to the historical nature of the area.

Irrigation – New irrigation will be installed according to the current COP standards and details. Irrigation will be provided to plant material only within the publicly owned right-of-way and/or landscape easement, and all associated equipment will also be located only in that same right-of-way/landscape easement. All existing, subsurface irrigation will be abandoned in place. Any existing irrigation equipment located at- or above-grade will be removed, including all valve boxes, backflow preventers, or other typical irrigation equipment.

Furnishings – New site furnishings will be installed as part of this project, to be located within the publicly owned right-of-way. The variety of furnishings will include trash receptacles, bike loops, and benches with backs. All site furnishings will match, in kind, the previously established standards used for the Copper Square district. Any furnishings installed south of Fillmore Street will include the copper-colored finish. All furnishings installed north of Fillmore Street will be of the same make and model, but of an alternative color, to be determined.

Signs – The introduction of interpretive or wayfinding signs is proposed throughout the project corridor. Signs of this nature are typically a custom fabrication, and at this time, no design suggestions are available.

Valley Gutter – The concept of new concrete valley gutters proposed for this project is to allow for the unobstructed flow of the stormwater associated with the 2nd Avenue roadway. Because of the introduction of landscape bays between parallel parking areas, it is necessary to install valley gutters between the parking areas and the nearest travel lane. Valley gutters will be utilized only in the section of 2nd Avenue between Adams Street and Fillmore Street (*see graphic sections*).

Crosswalk – New crosswalks are proposed at all pedestrian crossings. Crosswalks will be constructed and striped according to current COP and MAG standards and specifications. Subsequent to the Core Group recommendations, discussion with COP personnel regarding the Core Group recommendation for mid-block crossings resulted in City discouragement of the recommendation for safety reasons and for the lack of historical evidence of previously existing roadways at 2nd Avenue of the proposed mid-block crossing locations (present-day alignment of Polk and Taylor Streets). No mid-block crossings are recommended.

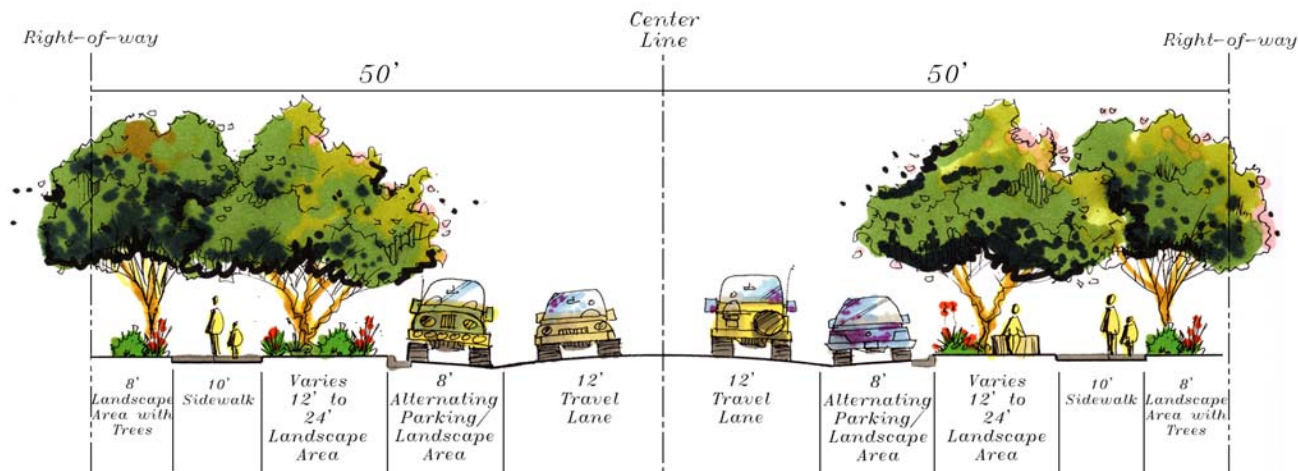
5.0 COST ESTIMATE

The following cost estimate (Table 4) is inclusive of all proposed improvements between Adams Street and Roosevelt Street within the 2nd Avenue right-of-way:

Table 4. ESTIMATED COSTS BY PROJECT ELEMENT – Adams Street to Roosevelt Street

Item	Unit	Qty.	Unit price	Total Amount
ACTUAL FUNDING (reported in Funding Application, 01)				
TRAFFIC CONTROL	LS	1	\$60,000.00	\$60,000
Allowance for Uniformed, Off-duty Law Enforcement Officer	LS	1	\$10,000.00	\$10,000
MISCELLANEOUS REMOVAL & OTHER WORK	LS	1	\$35,000.00	\$35,000
<i>Adjust Manhole to Grade</i>	EA	13	\$450.00	\$5,850
<i>Adjust Water Valve to Grade</i>	EA	9	\$250.00	\$2,250
LANDSCAPING + IRRIGATION				
<i>Tree (48" box)</i>	EA	295	\$850.00	\$250,750
<i>Shrub (5 gallon)</i>	EA	1,482	\$20.00	\$29,640
<i>Shrub (1 gallon)</i>	EA	780	\$10.00	\$7,800
<i>Accent (15 gallon)</i>	EA	370	\$75.00	\$27,750
<i>Accent (5 gallon)</i>	EA	620	\$30.00	\$18,600
<i>Decomposed Granite (3/8" screened)</i>	SF	103,772	\$0.50	\$51,886
<i>Irrigation</i>	LS	1	\$90,000.00	\$90,000
CONSTRUCTION COSTS				
<i>Remove Asphalt Concrete Pavement</i>	SY	8,063	\$9.00	\$72,567
<i>Subgrade Preparation</i>	SY	6,213	\$4.50	\$27,959
<i>Concrete Sidewalk (10', colored & textured)</i>	SF	62,898	\$5.00	\$314,490
<i>Remove PCC Curb & Gutter</i>	LF	3,625	\$5.00	\$18,125
<i>Remove PCC Sidewalk & Driveway</i>	SF	26,775	\$3.00	\$80,325
<i>New Concrete Driveway Entrance</i>	SF	35,069	\$6.00	\$210,414
<i>Remove Storm Drain Catch Basin</i>	EA	6	\$450.00	\$2,700
<i>New Storm Drain Catch Basin</i>	EA	12	\$1,500.00	\$18,000
<i>New Storm Drain Connector Pipe</i>	LF	80	\$75.00	\$6,000
<i>Remove and Salvage Existing Streetlights</i>	LS	1	\$6,000.00	\$6,000
<i>Street Lights at Intersections</i>	EA	10	\$4,500.00	\$45,000
<i>Pedestrian Lights (75' spacing, both sides) Copper Square fixture</i>	EA	86	\$5,000.00	\$430,000
<i>Trenching, Conduit, Backfill for Pedestrian Lights</i>	LS	1	\$44,100.00	\$44,100
<i>Milling (1 1/2")</i>	SY	7,551	\$3.00	\$22,653
<i>Asphalt Concrete Surface Course D 1/2, 1 1/2" Thick</i>	TN	638	\$65.00	\$41,470
<i>Asphalt Concrete Base Course, Type A 1 - 1/2, 4" Thick</i>	TN	345	\$65.00	\$22,425
<i>Asphalt Concrete Surface Course D 1/2, 1 1/2" Thick</i>	TN	131	\$65.00	\$8,515
<i>Concrete Valley Gutter</i>	SF	1,719	\$8.00	\$13,752
<i>Concrete Curb and Gutter</i>	LF	813	\$14.00	\$11,382
<i>Concrete Curb Bordering Landscaping Areas</i>	LF	3,625	\$14.00	\$50,750
STREET FURNITURE				
<i>Bench - 72" with back & center arm rest</i>	EA	36	\$3,000.00	\$108,000
<i>Trash Receptacle</i>	EA	36	\$1,200.00	\$43,200
<i>Bike rack (three loops/set)</i>	EA	7	\$600.00	\$4,200
<i>Special signage (kiosk)</i>	EA	2	\$6,000.00	\$12,000
SUBTOTAL				\$2,203,553
ALLOWANCE FOR EXTRA WORK	LS	1	\$60,000.00	\$60,000
PROJECT TOTAL COST				\$2,263,553

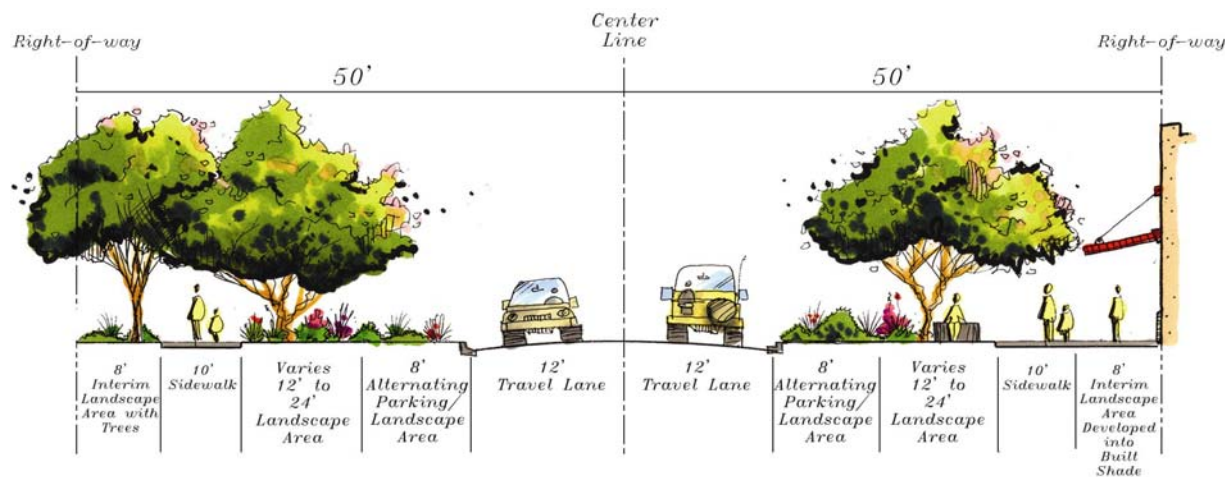
Cross Sections



*2nd Avenue Enhancement
Adams to Fillmore*

Depicting Parallel On-Street Parking and Interim Trees

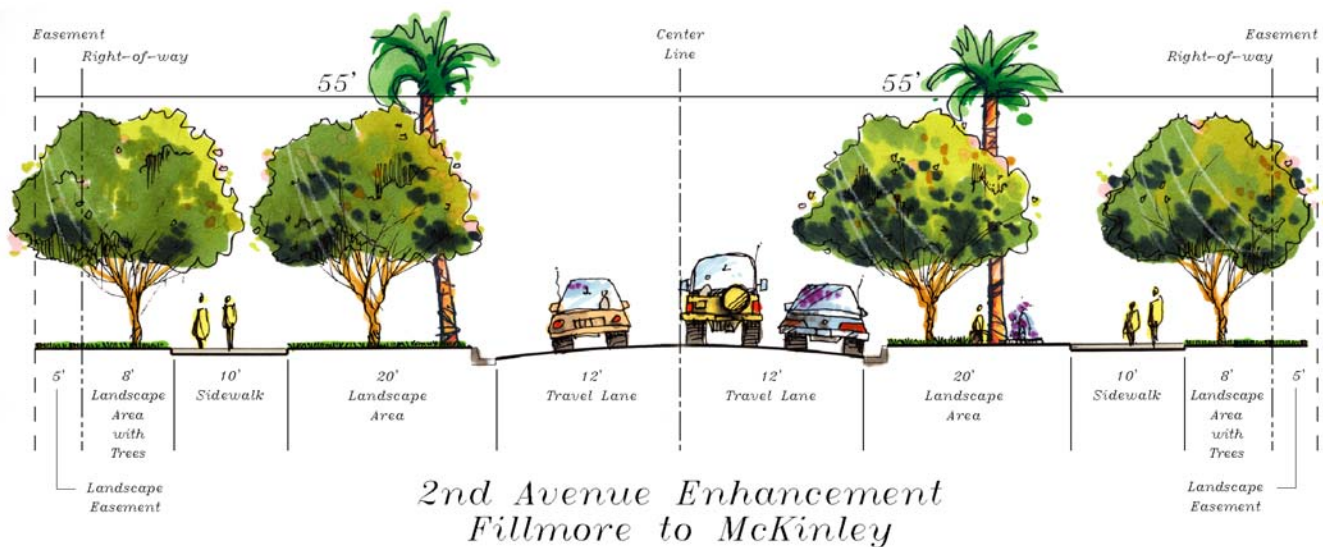
Exhibit 2. Section One. Typical section of the area between Adams Street and Fillmore Street. This section depicts the introduction of landscape bays, in place of parallel/diagonal parking facilities. The narrow road profile will encourage motorists to drive at a slower rate and increase the available space for additional landscaping and pedestrian areas and amenities. Interim landscape areas are encouraged to accommodate a double row of trees on either side of the sidewalk to provide adequate shade. View is looking north.



*2nd Avenue Enhancement
Adams to Fillmore*

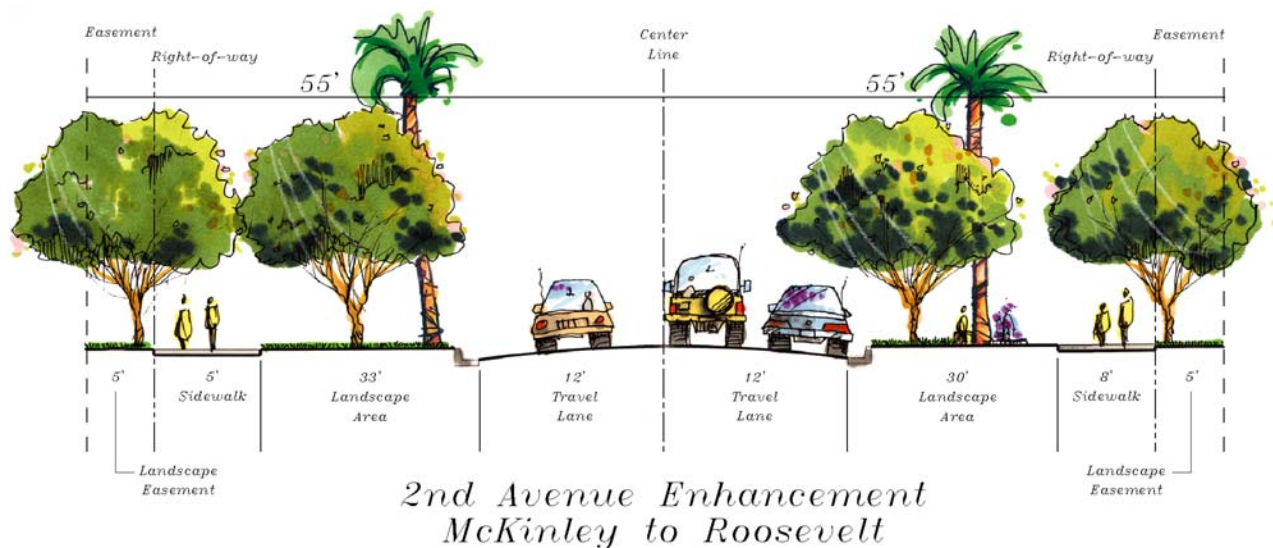
Depicting Landscape and Built Shade

Exhibit 3. Section Two. Typical section of the area between Adams Street and Fillmore Street. This variation depicts the construction of parallel parking facilities, in lieu of landscape areas. The parked vehicles add to the physical and visual separation between the pedestrians and travel lanes. The interim landscape area with the second row of trees may be removed as built shade is constructed with development to provide adequate sidewalk shade. View is looking north.



Depicting Double-row Tree Shade and Parallel Parking

Exhibit 4. Section Three. Typical section of the area located between Fillmore Street and McKinley Street. The physical roadway profile is virtually unchanged; however the pedestrian and landscape areas have been modified and improved. Note the small plaza space and the introduction of turf into the project. A double row of canopy trees provides adequate walkway shade. View is looking north.



Depicting Double-row Tree Shade and Parallel Parking

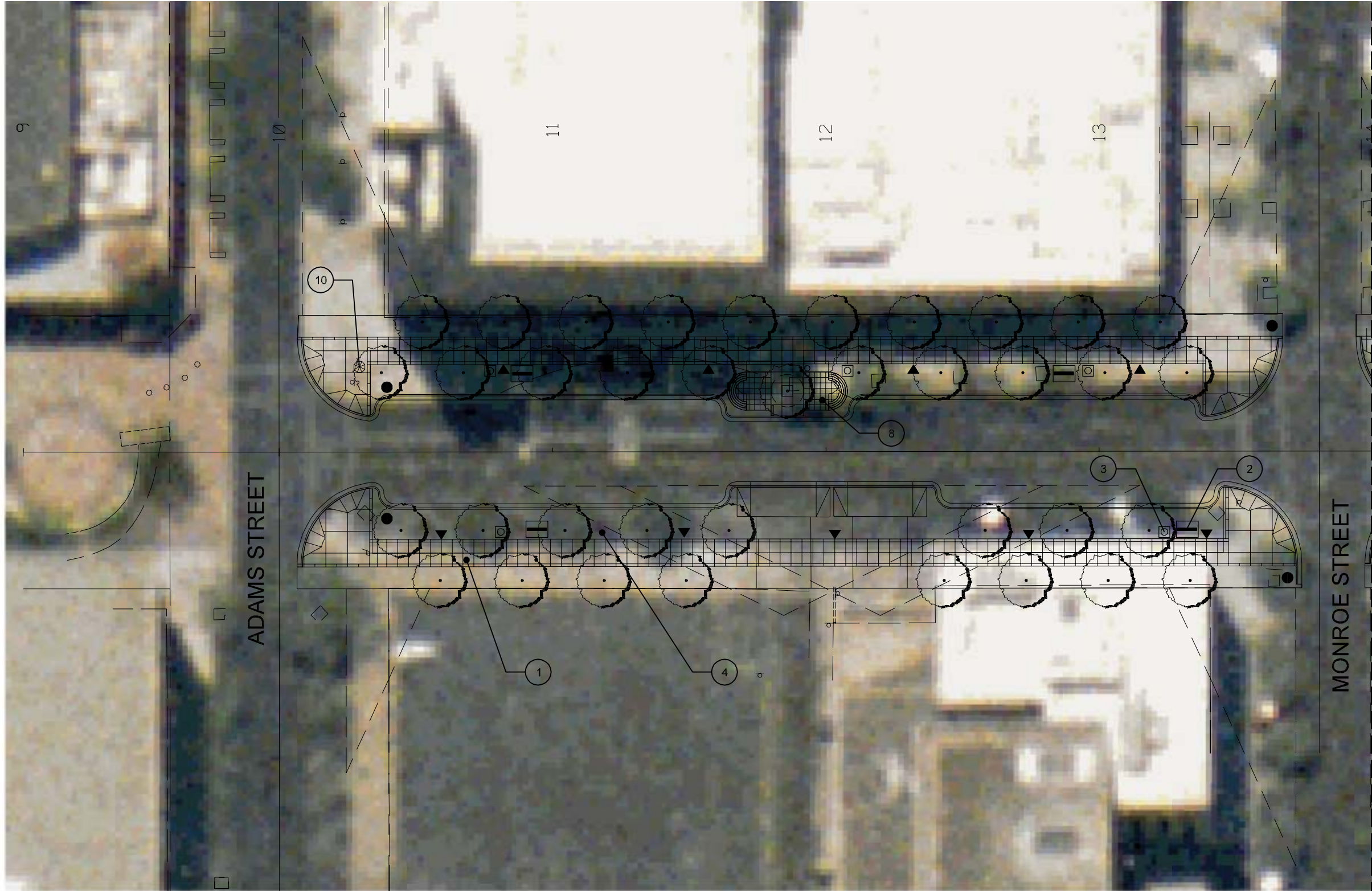
Exhibit 5. Section Four. Typical section of the area located between McKinley Street and Roosevelt Street. The physical roadway profile is virtual unchanged, however the pedestrian and landscape areas have been modified and improved. Note the existing turf in the project. A double row of canopy trees provides adequate walkway shade. View is looking north.

6.0 SCHEDULE

The City has been successful in obtaining a federal Transportation Enhancement Act-21 (TEA-21) grant as a source of partial funding for the design and construction of the 2nd Avenue enhancements between Monroe and Fillmore Streets. The second funding source for the segment between Monroe and Fillmore Streets will be the City of Phoenix itself. It is anticipated that the funded segment of the project, between Monroe and Fillmore Streets, will be under construction by federal Fiscal Year 2004 (January 2004) and completed before federal Fiscal Year 2005. The City of Phoenix is currently seeking additional funding for the 2nd Avenue segment between Fillmore and Roosevelt Streets.

7.0 PLAN SHEETS

The following plan sheets (Exhibits 6-A thru 6-G) graphically represent this document's final recommendations for 2nd Avenue between Adams Street and Roosevelt Street.



LANDSCAPE LEGEND

- Existing Palm Trees
- Canopy Tree
- Decomposed Granite
- Turf

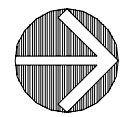
LIGHTING LEGEND

- Street Lights
- Pedestrian Lights

HARDSCAPE KEY NOTES

- 1 Integral Colored Concrete Sidewalk With Heavy Broom Swirl Finish, (TYP).
- 2 Straight Bench With Back And Center Armrest (TYP).
- 3 Trash Receptacle (TYP).
- 4 Landscape Planter (TYP).
- 5 Concrete Paver Sidewalk Extension (TYP).
- 6 Vender Area (TYP).
- 7 Landscape Easement
- 8 Seating Node
- 9 Existing Sidewalk
- 10 Kiosk (TYP).

MATCH LINE - SEE EXHIBIT 6-B PLAN SHEET







PRELIMINARY NOT FOR CONSTRUCTION OR RECORDING		MAG 2nd AVENUE PEDESTRIAN ENHANCEMENT ADAMS STREET TO ROOSEVELT STREET ALTERNATIVE REPORT	
LOGAN SIMPSON DESIGN INC		EXHIBIT 6-A PLAN SHEET	
SCALE: 1"=20'	DESIGNED BY: JK, AK	DATE: 08/2003	SHT. 24
	DRAWN BY: CS		

MATCH LINE - SEE EXHIBIT 6-A PLAN SHEET

VAN BUREN STREET

MATCH LINE - SEE EXHIBIT 6-C PLAN SHEET

LANDSCAPE LEGEND

-  Existing Palm Trees
-  Canopy Tree
-  Decomposed Granite
-  Turf

LIGHTING LEGEND

-  Street Lights
-  Pedestrian Lights

HARDSCAPE KEY NOTES

- 1 Integral Colored Concrete Sidewalk With Heavy Broom Swirl Finish, (TYP).
- 2 Straight Bench With Back And Center Armrest (TYP).
- 3 Trash Receptacle (TYP).
- 4 Landscape Planter (TYP).
- 5 Concrete Paver Sidewalk Extension (TYP).
- 6 Vender Area (TYP).
- 7 Landscape Easement
- 8 Seating Node
- 9 Existing Sidewalk
- 10 Kiosk (TYP).



PRELIMINARY
NOT FOR
CONSTRUCTION
OR RECORDING

MAG 2nd AVENUE PEDESTRIAN ENHANCEMENT
ADAMS STREET TO ROOSEVELT STREET
ALTERNATIVE REPORT

LOGAN SIMPSON DESIGN INC

EXHIBIT 6-B PLAN SHEET

SCALE:
1"=20'

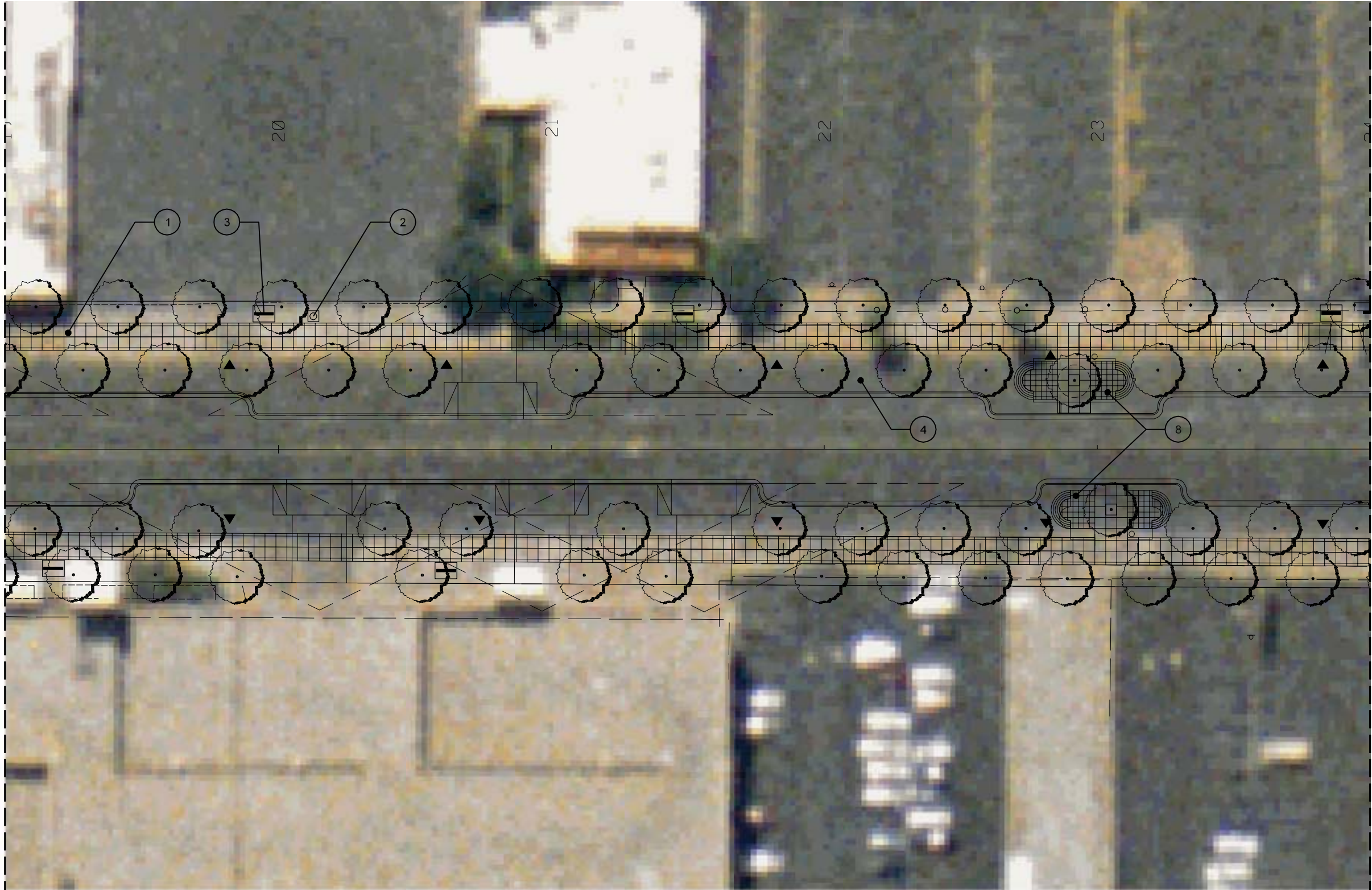
DESIGNED BY:
JK, AK

DATE:
08/2023

DRAWN BY:
CS





SHT.
25

MATCH LINE - SEE EXHIBIT 6-B PLAN SHEET





MATCH LINE - SEE EXHIBIT 6-D PLAN SHEET

LANDSCAPE LEGEND

-  Existing Palm Trees
-  Canopy Tree
-  Decomposed Granite
-  Turf

LIGHTING LEGEND

-  Street Lights
-  Pedestrian Lights

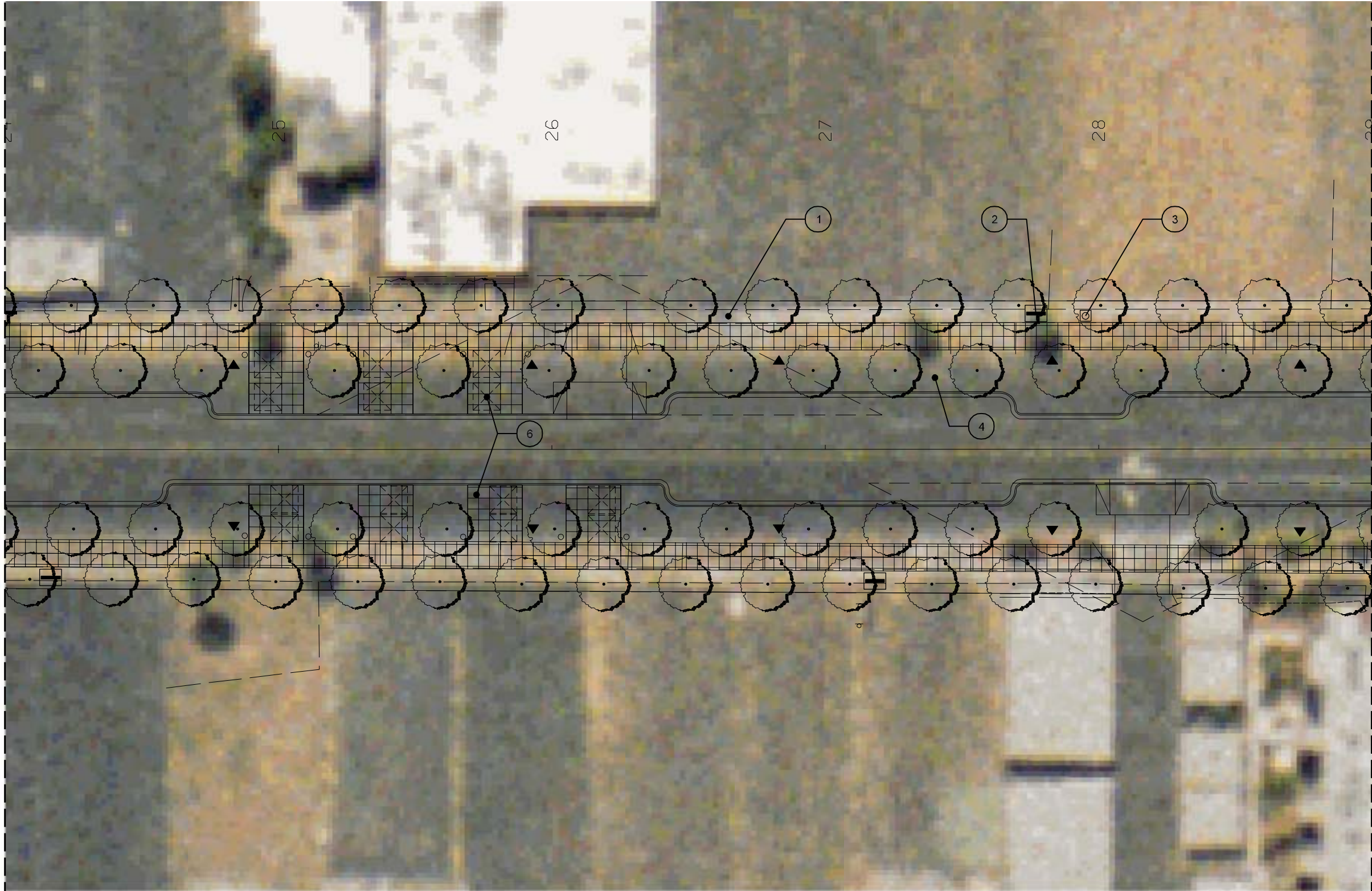
HARDSCAPE KEY NOTES

- 1 Integral Colored Concrete Sidewalk With Heavy Broom Swirl Finish, (TYP).
- 2 Straight Bench With Back And Center Armrest (TYP).
- 3 Trash Receptacle (TYP).
- 4 Landscape Planter (TYP).
- 5 Concrete Paver Sidewalk Extension (TYP).
- 6 Vender Area (TYP).
- 7 Landscape Easement
- 8 Seating Node
- 9 Existing Sidewalk
- 10 Kiosk (TYP).







PRELIMINARY NOT FOR CONSTRUCTION OR RECORDING		MAG 2nd AVENUE PEDESTRIAN ENHANCEMENT ADAMS STREET TO ROOSEVELT STREET ALTERNATIVE REPORT	
LOGAN SIMPSON DESIGN INC		EXHIBIT 6-C PLAN SHEET	
SCALE: 1"=20'	DESIGNED BY: JK, AK	DATE: 08/2023	SHT. 26
	DRAWN BY: CS		

MATCH LINE - SEE EXHIBIT 6-C PLAN SHEET





MATCH LINE - SEE EXHIBIT 6-E PLAN SHEET

LANDSCAPE LEGEND

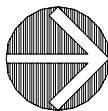
-  Existing Palm Trees
-  Canopy Tree
-  Decomposed Granite
-  Turf

LIGHTING LEGEND

-  Street Lights
-  Pedestrian Lights

HARDSCAPE KEY NOTES

- 1 Integral Colored Concrete Sidewalk With Heavy Broom Swirl Finish, (TYP).
- 2 Straight Bench With Back And Center Armrest (TYP).
- 3 Trash Receptacle (TYP).
- 4 Landscape Planter (TYP).
- 5 Concrete Paver Sidewalk Extension (TYP).
- 6 Vender Area (TYP).
- 7 Landscape Easement
- 8 Seating Node
- 9 Existing Sidewalk
- 10 Kiosk (TYP).



MATCH LINE - SEE EXHIBIT 6-D PLAN SHEET

29+61.38

FILLMORE STREET

10

1

7

2

3

4

7

1

Integral Colored Concrete Sidewalk With Heavy Broom Swirl Finish, (TYP).

2

Straight Bench With Back And Center Armrest (TYP).

3

Trash Receptacle (TYP).

4

Landscape Planter (TYP).

5

Concrete Paver Sidewalk Extension (TYP).

6

Vender Area (TYP).

7

Landscape Easement

8

Seating Node

9

Existing Sidewalk

10

Kiosk (TYP).

LANDSCAPE LEGEND



Existing Palm Trees



Canopy Tree



Decomposed Granite



Turf

LIGHTING LEGEND



Street Lights



Pedestrian Lights

HARDSCAPE KEY NOTES



AZTEC ENGINEERING

0 10 20 40 60

PRELIMINARY
NOT FOR
CONSTRUCTION
OR RECORDING

MAG 2nd AVENUE PEDESTRIAN ENHANCEMENT
ADAMS STREET TO ROOSEVELT STREET
ALTERNATIVE REPORT

LOGAN SIMPSON DESIGN INC

EXHIBIT 6-E PLAN SHEET

SCALE:
1"=20'

DESIGNED BY:
JK, AK

DATE:
08/2023

DRAWN BY:
CS





SHT.
28

MATCH LINE - SEE EXHIBIT 6-E PLAN SHEET

McKINLEY STREET

MATCH LINE - SEE EXHIBIT 6-G PLAN SHEET

LANDSCAPE LEGEND

-  Existing Palm Trees
-  Canopy Tree
-  Decomposed Granite
-  Turf

LIGHTING LEGEND

-  Street Lights
-  Pedestrian Lights

HARDSCAPE KEY NOTES

- 1 Integral Colored Concrete Sidewalk With Heavy Broom Swirl Finish, (TYP).
- 2 Straight Bench With Back And Center Armrest (TYP).
- 3 Trash Receptacle (TYP).
- 4 Landscape Planter (TYP).
- 5 Concrete Paver Sidewalk Extension (TYP).
- 6 Vender Area (TYP).
- 7 Landscape Easement
- 8 Seating Node
- 9 Existing Sidewalk
- 10 Kiosk (TYP).



PRELIMINARY
NOT FOR
CONSTRUCTION
OR RECORDING

MAG 2nd AVENUE PEDESTRIAN ENHANCEMENT
ADAMS STREET TO ROOSEVELT STREET
ALTERNATIVE REPORT

LOGAN SIMPSON DESIGN INC

EXHIBIT 6-F PLAN SHEET

SCALE:
1"=20'

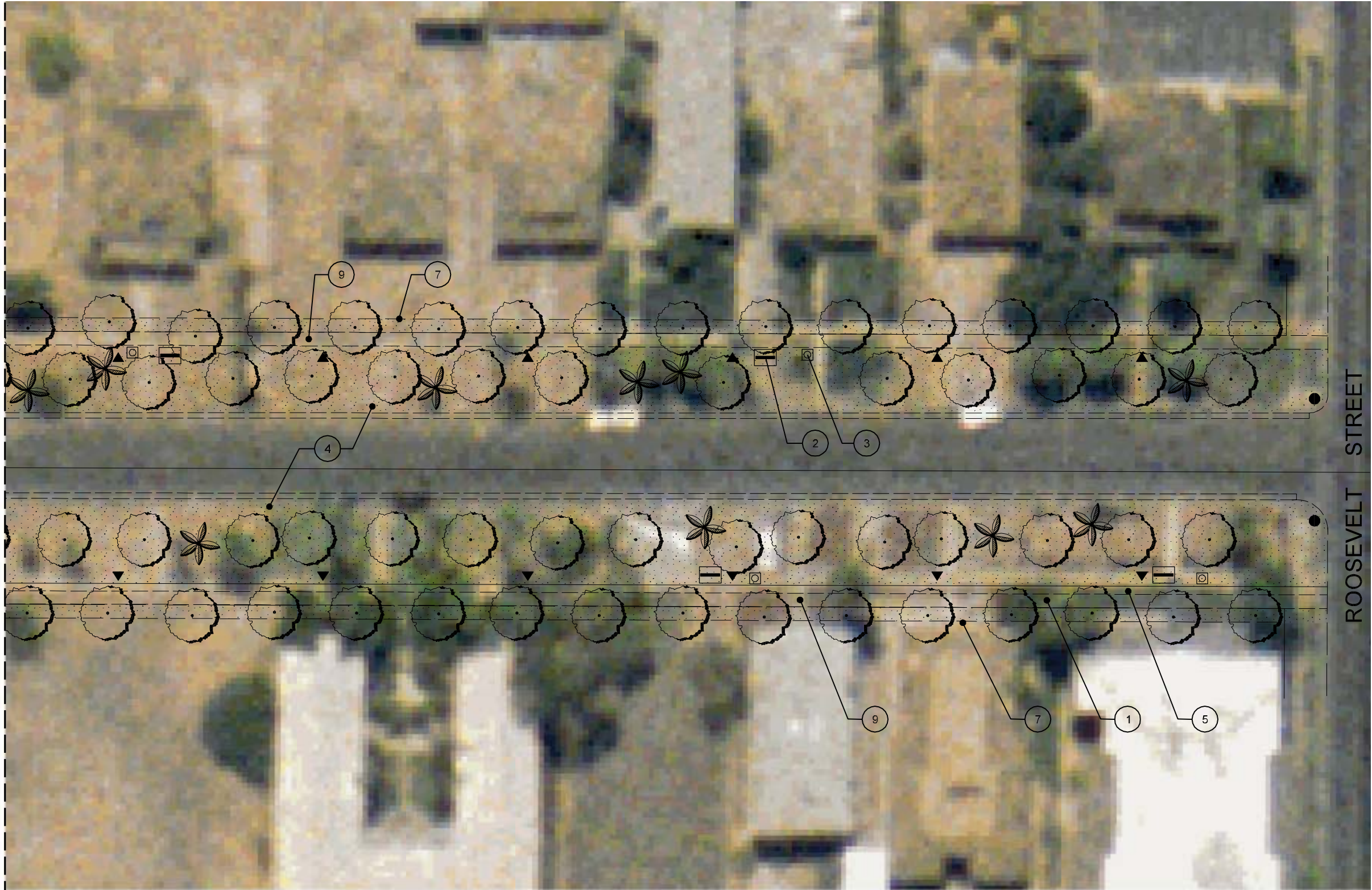
DESIGNED BY:
JK, AK

DATE:
08/20/23

DRAWN BY:
CS

SHT.
29

MATCH LINE - SEE EXHIBIT 6-F PLAN SHEET



LANDSCAPE LEGEND

- Existing Palm Trees
- Canopy Tree
- Decomposed Granite
- Turf

LIGHTING LEGEND

- Street Lights
- Pedestrian Lights

HARDSCAPE KEY NOTES

- 1 Integral Colored Concrete Sidewalk With Heavy Broom Swirl Finish, (TYP).
- 2 Straight Bench With Back And Center Armrest (TYP).
- 3 Trash Receptacle (TYP).
- 4 Landscape Planter (TYP).
- 5 Concrete Paver Sidewalk Extension (TYP).
- 6 Vender Area (TYP).
- 7 Landscape Easement
- 8 Seating Node
- 9 Existing Sidewalk
- 10 Kiosk (TYP).



PRELIMINARY
NOT FOR
CONSTRUCTION
OR RECORDING

MAG 2nd AVENUE PEDESTRIAN ENHANCEMENT
ADAMS STREET TO ROOSEVELT STREET
ALTERNATIVE REPORT

LOGAN SIMPSON DESIGN INC

EXHIBIT 6-G PLAN SHEET

SCALE:
1"=20'

DESIGNED BY:
JK, AK

DATE:
08/2023

DRAWN BY:
CS

SHT.
30

APPENDIX



March 5, 2003

Ashley Kowallis
Logan Simpson Design
51 West Third Street, Suite 450
Tempe, AZ. 85281

RE: FINAL DRAFT ALTERNATIVES REPORT - 2nd Avenue Pedestrian Enhancement

Dear Ms. Kowallis,

The City Historic Preservation Office has reviewed the Final Draft Alternatives Report received February 28, 2003. Historic Preservation staff has also visited the site and made the following observations:

- The concrete curbs, gutters, sidewalks, parkway (tree-lawn) landscape strip and the spatial relationship of these features along with the contractor's stamps in the concrete work are the primary remaining historic elements of the streetscape.
- That the installation of ramps or the raising of street elevations has previously modified the sidewalk/street juncture at all of the intersections. The ramps are not a historic feature and they have been installed in a variety of configurations.
- Driveway curb cuts have been installed in a number of configurations, but most with vertical curb-returns.
- There are a number of historic era contractor's stamps evident, even though most are not specifically dated.
- The contractor's stamps occur mid-block, near corners, on sidewalks and curb faces as well as on the gutters near some of the storm drains.
- At least one example of a historic era "City of Phoenix Irrigation Department" irrigation control box exist at the northeast corner of McKinley and 2nd Avenue.
- Curb heights gradually increase from the north, at Roosevelt Street, to the south towards Adams Street.
- The wide 14-foot parkway (tree-lawn) landscape strip exists between Roosevelt and Fillmore Streets.
- South of Fillmore Street the sidewalk is adjacent to the curb, gutter and paving.
- There are no remaining historic era street light fixtures or other street fixtures or furniture.

The City Historic Preservation Office has the following comments on the Final Draft Alternatives Report:

1. In the Roosevelt Historic District (north of McKinley Street), 10-foot wide walkways are not typical of the historic residential neighborhood. Increasing the width of the sidewalks from 4 feet to 10 feet will reduce the historic parkway (tree-lawn) landscape strip from 14 feet to only 8 feet. Within the Roosevelt Historic District the existing sidewalks should be repaired and maintained as a historic element with no

1200 West Washington Street, 17th Floor • Phoenix, Arizona 85003 • 602-261-8699 FAX: 602-534-4571

Recycled Paper

additional width. This will help to preserve the integrity of the historic district's design, setting, feeling and association with our past.

2. Bulb-type curb extensions should not be considered for 2nd Avenue from the north side of McKinley Street to Roosevelt Street. Bulb-type curb extensions will reduce the historic integrity of the streetscape setting.
3. It appears that the proposed project will destroy most of the historic period contractor's stamps in the sidewalks and curbs. This is especially true in the area south of Fillmore where the street is to be narrowed. New access ramps at intersections will destroy others. Every effort should be made to avoid removal and preserve these stamps in place, as evidence of the City's historic past, even though many are not dated. Stamps should not be cut out and re-installed in new sidewalks. If preservation in place is not possible, examples of stamps could possibly be removed by saw cutting and used in an interpretive historical exhibit.
4. Seating areas in the Roosevelt Historic District north of McKinley (Community Area) and should provide seating of the bench-type rather than the integral bench and wall-type that is not characteristic of historic residential areas.
5. Historic interpretive signage should be incorporated as part of the enhancement project. The signage should be incorporated into the streetscape and seating areas adjacent to or near the remaining individual historic structures south of McKinley Street. This should include the following buildings:
 - Herman Demund House (Law Office), 649 N. 2nd Avenue.
 - Rincon/Casa Marvel Apartments (The Lofts at Fillmore), 387 N. 2nd Avenue.
 - J.T. Whitney Funeral Home (Law Office), 330 N. 2nd Avenue.
 - Orpheum Theatre, 209 W. Adams.
 - The 1895 House, 362 N. 2nd Avenue.
6. Informal seating areas with interpretive signage should be provided within the City right-of-way adjacent to or near the remaining individual historic properties. Signage should be specific to the near-by historic property.
7. Interpretive signage relating to the Roosevelt Historic District should be incorporated into the streetscape north of McKinley Street.
8. The City Historic Preservation Office encourages traditional green landscaping composed of turf, shrubs and trees in the community portion of the streetscape north of Fillmore. This is especially the case in the Roosevelt Historic District, north of McKinley.
9. The historic era "City of Phoenix Irrigation Department" irrigation control box at the northeast corner of McKinley and 2nd Avenue should be preserved and interpreted historically.
10. New concrete work should be date stamped. This will illustrate the progression of development and aid in the interpretation of history.

In addition the Historic Preservation Office concurs with the following specific Core Group Recommended Policies:

- P9 Match setbacks to those of existing historic properties in the Community Area (north of Fillmore).... *In particular the Historic District north of McKinley.*
- P12 Encourage adding openings or other visual interest/animation on existing buildings that do not have a high proportion of openings in the building facades .. *This should not encourage similar additions or alterations to historic structures*

- P15 Provide policies or stipulations that retail entrances should front onto pedestrian routes.
- P18 Interpret existing and former historic aspects of the Corridor....
- P19 Retail and all development along the Corridor in the District Area (south of Fillmore Street) should be encouraged to have a length of no greater than 35 feet...

- P21 Provide interpretive sign package design guidelines.
- P32 Undertake a descriptive brochure, a video, oral histories project....
- P33 A system of interpretive signs should be implemented ... Celebrate historically significant properties and properties that no longer exist....
- G3 Provide a landscaped buffer that matches the existing adjacent buffer in the Community Area (north of Fillmore Street)...
- G20 Support existing guidelines and requirements to orient building entries toward 2nd Avenue...
- G21 Support existing guidelines and requirements to establish a strong sense of entry into each building.
- G23 In the Community Area (north of Fillmore) provide trash receptacles at nodes. The choice of trash receptacle3s in the Community Area should respect the area's historic character.
- G27 In the Community Area (north of Fillmore Street) provide 75 percent shade in the right-of-way with mainly vegetation ... *Shade structures are not encouraged in the Roosevelt Historic District north of McKinley.*
- G33 In the Roosevelt Historic Neighborhood, use a street light fixture that respects the historic character of the neighborhood.
- G35 Provide interpretive signs that respect the historic character in the Community Area (north of Fillmore).

Attach are examples (photographs) of historic era contractor's stamps. This is not intended to be a complete inventory. The dates of most are unknown, but they appear to be original to the early concrete work. Include is a photo of the "City of Phoenix Irrigation Department" irrigation control box.

Included, also is a 1957 City of Phoenix date stamp that is not yet 50 years old, but does date the more recent sidewalk replacement work. Other replacement work is dated 1982 and 2000.



Barbara Stocklin
City of Phoenix
Historic Preservation Officer

2nd Avenue Enhancement Project

Potentially Historic Concrete Stamps

Page 1



Photo 1. CEMENT WORKERS CONCRETE CO - 1909. East side, mid-block, Fillmore to McKinley & SE corner at McKinley.



Photo 3. CEMENT WORKERS CONCRETE CO. McKinley to Roosevelt, mid-block.



Photo 2. ARIZONA-CALIFORNIA, INSPECTED BY CITY, CONSTRUCTION CO. NE corner at McKinley & west side, mid-



Photo 4. ARIZONA-CALIFORNIA, INSPECTED BY CITY, CONSTRUCTION CO. East side, mid-block, Van Buren to Fillmore.

2nd Avenue Enhancement Project
Potentially Historic Concrete Stamps
Page 2

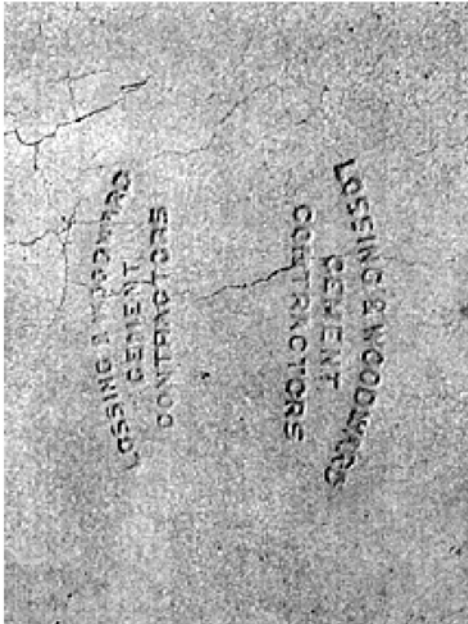


Photo 5. LOSSING & WOODWARD CEMENT CONTRACTORS.
SW corner at Roosevelt & mid-block McKinley to Roosevelt.

2nd Avenue Enhancement Project

Potentially Historic Concrete Stamps

Page 3

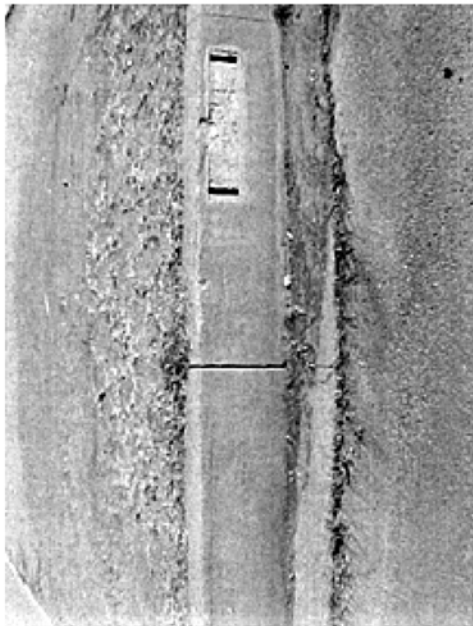
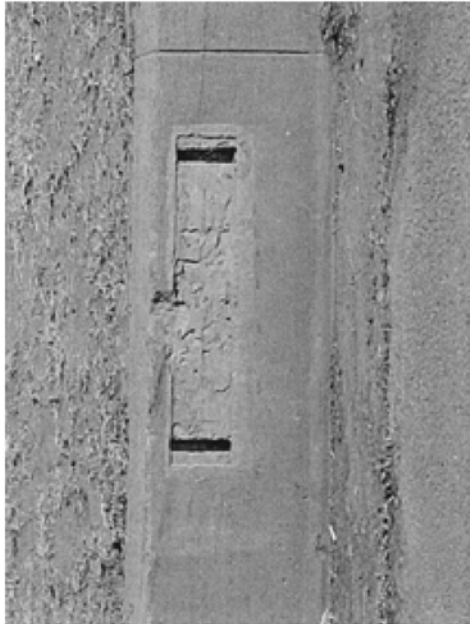


Photo 6. Concrete stamp & earlier street name sign location. East side of 2nd Avenue.



corner

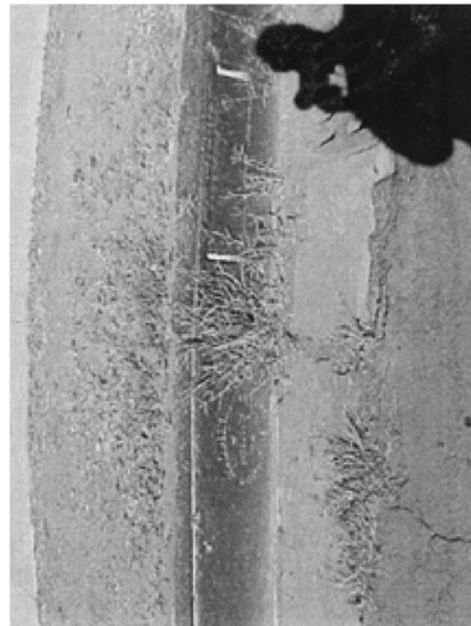


Photo 8. SOUTHWESTERN PAVING CONTRACTING COMPANY. Street name sign location. NW corner at Fillmore.



Photo 9. SOUTHWESTERN PAVING CONTRACTING COMPANY. North & south sides of Fillmore at 2nd Avenue.

2nd Avenue Enhancement Project

Potentially Historic Concrete Stamps

Page 4



Photo 10. CITY OF PHOENIX IRRIGATION DEPARTMENT.
NE corner at McKinley.

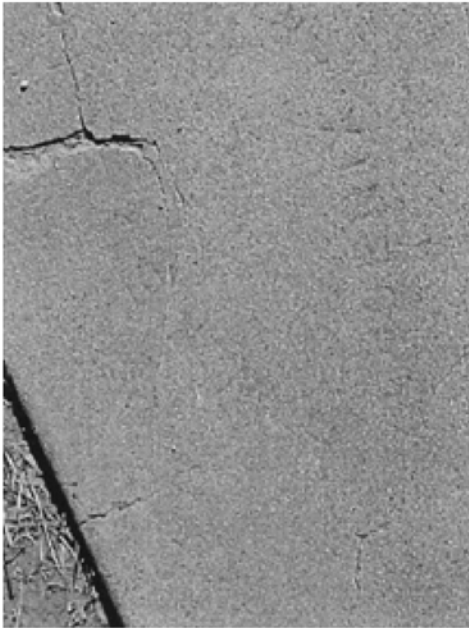


Photo 11. Unreadable stamp. __W_K__ING__, PHOENIX
ARIZONA. NE corner at McKinley.



Roosevelt.



Photo 13. PhotCITY OF PHOENIX DIV. OF STREET MAIN. 1957.
West side McKinley to Roosevelt & East side Van Buren to Fillmore.
LESS THAN 50 YEARS OLD.